



# ORMOND ROAD URBAN DESIGN GUIDELINES

August 2008



## FOREWORD

### Differences between the Guidelines and the Planning Scheme Provisions (DDO18)

The Ormond Road Urban Design Guidelines form the basis for the controls specified in the Design and Development Overlay (DDO18) at Clause 43.02 of the Port Phillip Planning Scheme. The DDO18 controls have been implemented through Amendment C57 – Part 1 and Part 2, to the planning scheme.

There are some discrepancies between the height specified in the Guidelines and the DDO18. The discrepancies between the Guidelines and the DDO18 occurred as a result of the Panel's consideration of Amendment C57 and C57 Part 2, and Council's subsequent consideration of the recommendations of the Panel for these amendments.

## INTRODUCTION

Background	1
Purpose and Scope	1
Project Area	1
Consultation	1
Strategic planning Context	2
Prescriptive and Performance Controls	3

## ANALYSIS

Character Areas	5
Retail Precinct 1	6
Residential Precinct 1	7
Retail Precinct 2	8
Residential Precinct 2	9
Typical Street Cross-Sections	10
Existing Building Heights	11
Heritage Overlays	12
Public Transport	13
Level of Development Opportunity	14

## GUIDELINES

Retail Precincts - Introduction	16
Retail Precincts - General Guidelines	17
Retail Precincts - Corner Lot Guidelines	18
Retail Precincts - Possible Future Development	19
Residential Precinct - Introduction	20
Residential Precinct - Guidelines	21

## Background

Elwood is a neighbourhood activity centre servicing local shopping and business needs. A range of retail, entertainment, cafes and restaurants and service business activities service the local population.

The centre is divided into Elwood Village, on Ormond Road, surrounded by residential development with additional retail opportunities located at the corner of Ormond Road and Glenhuntly Road some 350 metres away. Residential development separates the two retail areas. Both are traditional strip shopping centres bisected by the street.

Currently there is pressure for larger scale (5-storey) residential development. Despite Council's refusal, VCAT has directed the issue of permits to allow their development.

## Purpose and Scope

The purpose of this project is to provide urban design guidelines for built form within the centres and the residential land in between, that provide clear recommendations and a credible and sound basis for subsequent interim planning controls and/or the design element of a future Structure Plan.

The guidelines are based on an assessment of the urban design 'values' of Elwood, including a townscape analysis of the project area and its immediate surroundings. They consider state and local planning policy and strategies (particularly *Melbourne 2030*), likely development forms based on lot sizes and current market preferences, and the potential impacts of buildings on:

- Local built form character
- Heritage
- The amenity of the public realm (Glenhuntly and Ormond Roads)
- The amenity of adjoining private properties

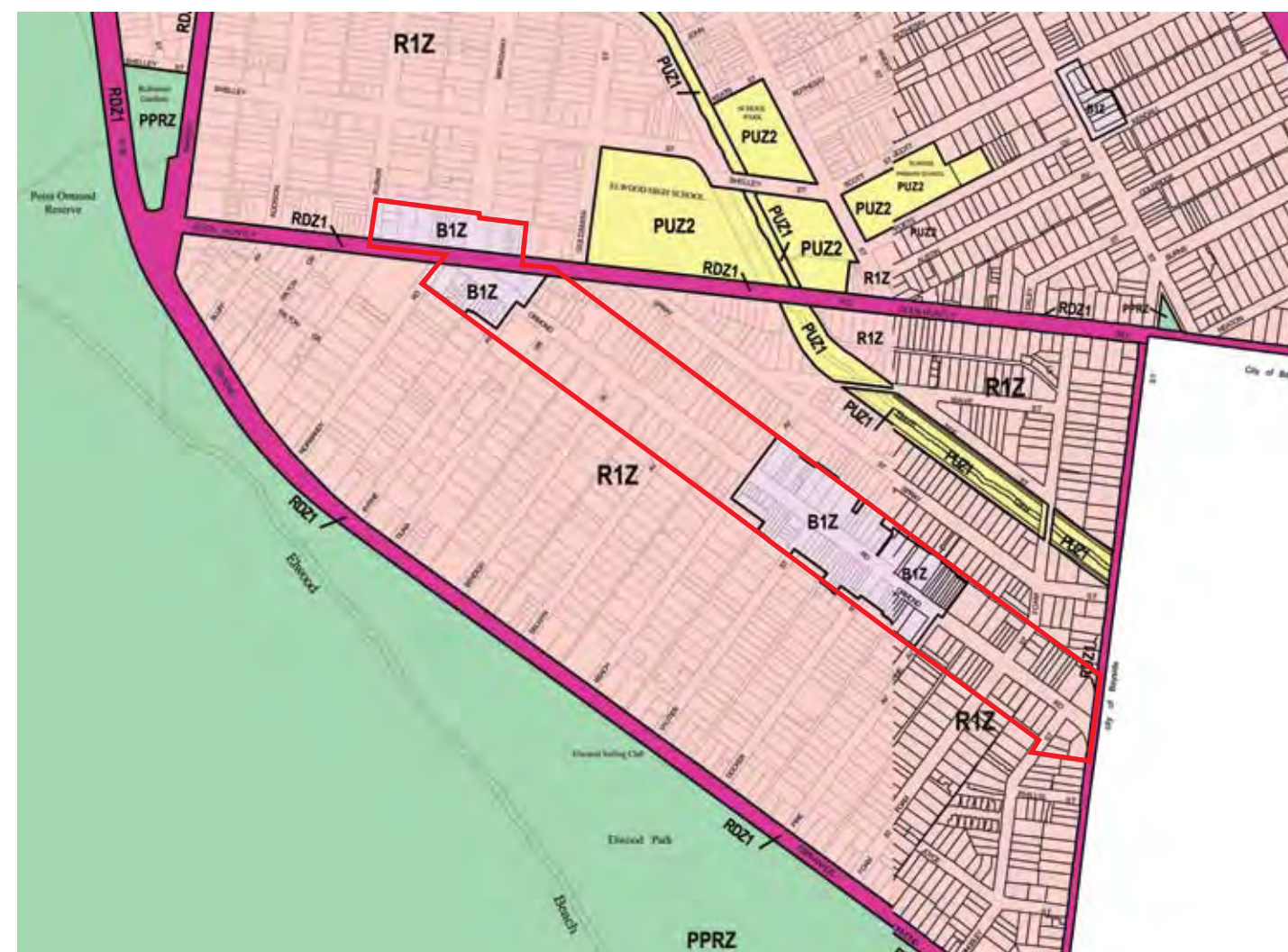
The guidelines clearly identify the appropriate form of development in each part of the area, including a strategically justified height limit.

## Project Area

Recommendations focus on the Business 1 zone precincts on Ormond Road and around the Glenhuntly Rd/ Broadway/ Ormond Rd intersection, as well as the Residential 1 zone precincts along Ormond Road, between the 2 business hubs and between the Ormond Road hub and St. Kilda Street (see plan opposite). Analysis has extended beyond this area as necessary to provide a strategic basis for future controls.

## Consultation

The Guidelines also take into account the concerns and aspirations of the local community in relation to the centre, as expressed by those that attended community meetings held on 9 February and 14 March 2006.



## Strategic Planning Context

Planning is undertaken within a State policy and legislative context, both of which must be implemented at the local level by the Council. *Melbourne 2030 (M2030)* sets out the State's policy for the next 30 years and the Planning and Environment Act 1987 specifies the legal framework within which decisions are made and details the processes Council is required to follow.

Elwood is identified under *M2030* as a neighbourhood activity centre servicing local shopping and business needs.

Direction 1 of *M2030* is "A More Compact City" that stipulates building up Activity Centres as a focus for high-quality development, diversification of the services provision and location of a substantial proportion of new housing within or in close proximity. It has been used by developers to justify more intensive, particularly residential, development in many centres across Melbourne.

*M2030* is not only about urban consolidation but also the creation of a quality urban environment. Direction 5 is "A great place to be" which identifies 'sense of place', 'liveable environments', 'cultural identity', 'community building', 'cultural vitality', and 'the values, needs and aspirations of the community' as important components of the environment and even includes an initiative to: "strengthen tools in the planning system to ensure development responds to its context in terms of built form, landscape character and cultural identity".

The City of Port Phillip's (CoPP) Municipal Strategic Statement sets out council's strategic planning objectives, based on input from the community, and underpins the land use and development provisions in the planning scheme. Policies specific to Elwood are set out below:

### Clause 21.04-3 Elwood and Ripponlea

In Elwood and Ripponlea the Council envisages:

- Established residential areas retain their distinctive suburban character including large front and rear setbacks, established gardens and low rise building form. There is limited new development and established homes are encouraged to be retained and refurbished to ensure that a range of housing choices are available.
- Marine Parade and Ormond Esplanade retain their residential character. Existing dwellings and apartment buildings are upgraded and, except for identified heritage places replaced with new single houses and multi-unit developments. A high standard of design that responds to the character of the area is achieved in recognition of the prominence of this area along one of the major seaside boulevards in the municipality.
- The Elwood Junction, Elwood Village, Ripponlea and Tennyson Street retail centres continue to be key hubs of local community activity offering a balanced range of retail shops and some eating premises. New residential development is encouraged in all centres above and behind retail frontages, respectful of the existing scale and character of these areas. The interface between the retail centres and surrounding residential areas is carefully managed to minimise loss of amenity to nearby residents.

### Clause 21.05-1

Elwood and Ripponlea Neighbourhood:

- Discourage new medium density development in established residential areas, except in appropriate locations such as on main roads or public transport routes if no other constraints exist such as heritage considerations.
- Protect the remaining stock of large dwellings and single dwelling lots in order to offer a range of housing options to the community.
- Protect the residential character of Marine Parade and Ormond Esplanade.

### Options for Study Area

Under the above context, the following options are available in relation to development controls for the Study area:

#### 1. Option 1 - No Strategic Framework

Without a strategic framework in the Planning Scheme, developments incongruous with the character of the area may take place using previous proposals passed through VCAT as precedents.

#### 2. Option 2 - Interim Height Limits

The Council could make use of the provision of a Design and Development Overlay (DDO) 'Interim control' that establishes height limits for neighbourhood centres. However, the DDO schedule is a "fixed" schedule that offers a height control only (maximum metres) precluding finer design criteria such as street wall heights, setbacks and materials that impact on streetscape. Also, this control must be proven consistent with *M2030* and will expire on 31 December 2007, beyond which the Council will be obliged to work towards putting permanent controls in place.

#### 3. Option 3 - Undertake preliminary work as basis for interim control

A more strategic alternative would be to determine the urban design 'values' of the Study Area and professionally calculate an appropriate level of development. These Guidelines provide such an outcome, and would provide a credible basis for planning controls.

#### 4. Option 4 - Structure Plan

A townscape analysis, which is ultimately required to be undertaken, would lead to a justifiable height control, but would not allow application of a more sophisticated interim control. Further, this is a large-scale job, requiring greater commitment of time and resources. Tailor-made interim controls must be based on reasonable progress towards a structure plan, including some community consultation.



### The Planning Process

An interim control requires a “Ministerial” amendment. The process is:

1. Council makes application to the DSE / Minister
2. If supported, the Minister gazettes an amendment with no consultation

The timeframe is unknown and beyond Council's control.

A permanent planning control involves the following steps:

1. Request to the Minister to authorise Council to exhibit the amendment
2. Exhibition to all affected property owners and occupiers
3. Council considers submissions and requests a Panel be appointed
4. Panel considers submissions and the amendment and makes recommendation back to Council
5. Council considers Panel report and may adopt, change or abandon amendment
6. Council forwards amendment to DSE where it makes its way, subject to officer recommendation, to the Ministers office for approval (signing)
7. Amendment gazetted and takes effect.

The amendment timeframe as outlined above generally takes in the order of 12 months for Council to get it through the process. The length of time with DSE / the Minister is variable.

### Prescriptive and Performance Controls

Prescriptive controls in land use planning are based on predetermined absolute standards. These types of controls provide fixed directions to planners for decision-making in land use and building regulation. Such controls could be applied to industrial standards, building codes, or building design standards like heights, setbacks, floor areas, building materials etc. Prescriptive controls have been actively criticised for their inflexible, narrow focus and “blunt” approach to land use. Top-down prescriptive regulatory mechanisms have often resulted in extremely uniform outcomes, diminishing the diversity of places. They also preclude the opportunity to respond appropriately to unusual circumstances.

Performance-based planning controls have been used in an attempt to create better places by increasing the flexibility of decision-making in land use control and building regulation. Performance-based approaches set standards that describe the desired end result and acceptable limits of impact. This type of regulation offers the decision-maker the discretion to respond to the site and application on its merits, requires fewer regulations and, most importantly, encourages greater dialogue amongst stakeholders.

The Victorian land use planning system is largely performance-based with room for incorporating prescriptive controls as needed.



The following drawings illustrate the key analyses undertaken to inform the Guidelines. These include:

### Character areas

Existing built form character is one of the key factors used to inform the development of these Guidelines. A relatively coherent character, is considered to be an important value in determining the preferred future character.

Key elements of existing character in each precinct (other than building height, mentioned below) are:

- Both retail precincts are dominated by buildings with masonry external walls, punched windows, parapets, traditional active-fronted shopfronts built to and for the full width of the street boundary, and verandahs. Shop-top housing is also a characteristic feature of the area. The precinct at the intersection of Glenhuntly and Ormond Roads, Elwood Junction, is dominated by buildings from the Inter-war period with high floor-to-floor heights, and a number of corner features. The other precinct, Elwood Village, has a smaller proportion of Inter-war buildings. It includes a mix of single and double storey terraced shops with a few free standing buildings on larger lots.
- Both residential precincts are characterised by detached dwellings and walk-up flats dating from the Inter-war period. These have small front and side setbacks, larger rear setbacks, and front garden vegetation. The streets are lined with a substantial number of trees.

The Port Phillip Design Manual (2000) provides a comprehensive urban character assessment and policy statement for the Study Area in its 'Urban Character Description' of areas 3,4 and 5. It identifies important and very important character elements to which all proposed building designs should respond.

### Existing building heights

As noted above, existing character is one of the key factors used to inform the development of these Guidelines.

- A key element of built form character is building height. This analysis shows that:
- The retail precinct at the junction of Glenhuntly and Ormond Roads comprises predominantly 2-3 storey buildings, contributing to a relatively coherent built form character.
- The residential precinct between the two retail precincts comprises a mix of single-storey houses and 3-storey blocks of flats, with a small number of 2-storey buildings, contributing to a mixed built form character in which the blocks of flats dominate due to their size.
- The retail precinct on Ormond Road comprises predominantly single and 2-storey buildings, with a small number of 3-5 storey buildings, contributing to a mixed built form character.
- The residential precinct at the southeastern end of Ormond Road comprises predominantly single-storey houses, with 3-storey buildings at either end, contributing to a relatively coherent built form character.

### Heritage overlays

The Study Area is affected by Heritage Overlay 8, which covers large parts of Elwood, and Overlays

220, 221, 303, 314 and 435, which apply to specific properties. Heritage Overlay 219 applies to St Columbas Church and School, which abut the Study Area. The Heritage Overlays shown on page 12 include the Interim control gazetted by the Minister in April 2006, while Amendment C54, which implements the Elwood Heritage Review, is subject to the proper planning processes.

According to the Port Phillip Heritage Review (version 3, 2005), the area has historical value for its capacity to demonstrate the attraction of the Port Phillip Bay coastline as a location for marine villas during the mid-late Victorian period. The aesthetic value of its diversity of villas and Inter-war apartments, and its tree lined streets, are distinctive characteristics.

There are relatively few buildings representing the early history of the area. It is therefore important that high regard is had for these buildings. While the heritage overlays do not preclude development in all cases, they may constrain the extent of potential development on those properties. They also indicate properties with heritage values that adjoining development should have regard to.

### Public transport

One of the key concepts underpinning Melbourne 2030's activity centres policies is to concentrate development close to public transport to reduce the use of and reliance on private cars for transport.

The Study Area is relatively poorly served by public transport compared with many other activity centres. It has no train or tram services, and neither of its bus services provide direct connections to central Melbourne where many of the Study Area's residents might be expected to work.

The transport benefits to be gained by concentrating development in and around the Study Area are therefore limited.

### Development potential

The development potential analysis indicates the likelihood of each property being redeveloped, based on an analysis of the factors that may constrain development, including:

**Age and height of existing development** - newer and taller buildings usually indicate a higher current capital value that has to be exceeded before redevelopment is viable

**Strata titles** - strata-titled properties generally need to be amalgamated before redevelopment can occur, which is often difficult to achieve

**Lot size** - small lots are difficult to develop economically for multi-level buildings, due to inefficiencies in car parking and vertical circulation, and the amalgamation of lots is relatively rare in such centres

**Heritage values** - heritage values often constrain development potential, as noted above

The analysis shows that substantial parts of the Study Area have low development potential due to heritage values, small lot sizes, strata titling or recent taller development. This is particularly true in the two retail precincts and suggests that if a change of built form character is envisaged, it would take a long time to be realised throughout a whole precinct, resulting in an extended transition period with an incoherent character.

There are two larger areas - both residential - in which most properties have moderate to high development potential: on Ormond Road between Byrne Avenue and Bendigo Avenue, and at the southeastern end of Ormond Road. There are also a number of scattered clusters of higher development potential generally limited to 4-5 properties. A change of character in these areas could happen more quickly.

Note that the purpose of this analysis is to understand the likely pattern of redevelopment. It is not intended to limit the development potential of individual properties.



# CHARACTER AREAS

### Retail Precinct I- Elwood Junction

- This precinct surrounds a large roundabout dominated by mature gum trees, which provide an attractive focal point.
- It has a juxtaposition of fine commercial and public buildings, giving it landmark value.
- It falls within Business 1 zone and is affected by a Heritage Overlay.
- The dominant scale of buildings is two storeys, built to and for the full width of the street boundary.
- Traditional strip shops at street level and shop-top housing constitute the land uses.
- The street is highly active with outdoor cafes, dining places and shaded roadside walkways.

### Residential Precinct 1

- This precinct falls in the Residential 1 zone.
- Parts of it are under a Heritage Overlay.
- It consists of detached dwellings and walk-up flats from the Inter-war period.
- It has a mixed character of single, double and triple storey buildings with varying front setbacks.
- Large deciduous trees and shrubbery lining the streets and low height front fences contribute to its character.

### Retail Precinct 2

- This precinct falls in the Business 1 zone and parts of it are under a Heritage Overlay, underscoring the area's distinctive Inter-war character.
- It has a mixed character of single, double and triple storey buildings abutting the street. A few new four-five storey buildings add to the mix.
- It is a traditional strip shopping centre.
- All buildings about the road except a few free-standing apartment buildings on larger lots.
- In general, the lot sizes on the south side are markedly narrower than those on the north.
- The street is highly active with outdoor cafes and dining places and shaded roadside walkways.

### Residential Precinct 2


- This precinct falls in the Residential 1 zone.
- Parts of it are under a Heritage Overlay.
- A high proportion of the buildings are deemed either Significant or Contributory heritage places within the Heritage Overlay while most buildings outside it are identified as contributing to the neighbourhood character.
- It is dominated by single-storey detached dwellings with pitched terracotta roofs. Three-storey developments occur at street corners.
- Large trees and shrubbery along the street further define its character.

**PRECINCTS**

- RETAIL
- RESIDENTIAL

PPH005 / 001

Scale 1:2500



# RETAIL PRECINCT 1



**Retail Precinct 1- Elwood Junction**  
 This precinct is a traditional strip shopping centre. It is characterised by:

- Business 1 zoning
- A Heritage Overlay
- Varied lot sizes
- Predominantly two-storey buildings, with 'traditional'/ higher floor-to-floor heights.
- Buildings abutting the street with most providing 'active' shopfronts and verandahs
- Buildings with masonry external walls, punched windows and parapets
- Shop-top housing
- Outdoor cafes and other dining places
- Juxtaposition of fine commercial and public buildings giving it landmark value - particularly the tower of St Columbas Church
- Heritage buildings dating back to 1900



- A new 3-storey corner development with height similar to heritage buildings, incorporating new street level cafe and upper floor apartments

- New development in scale with heritage buildings with contrasting contemporary character
- More outdoor dining places



- "The Alderly", one of the key heritage buildings at the Junction, adds aesthetic value to the precinct with its distinct style and sets the character of the precinct as an Inter-war centre of civic distinction
- Outdoor dining spaces add to the character of the junction



- The St. Columbas Church - a local landmark

- Large roundabout with mature gums contributes to the character of the area
- However, the street detailing and other landscaping is inconsistent with the heritage qualities of the area.



- "The Alderly" with its twin oriel towers responding to its corner situation
- Heritage buildings provide a coherent street character on both sides of Ormond Road.



PPH005 / 002  
 Scale: N.T.S



# RESIDENTIAL PRECINCT 1

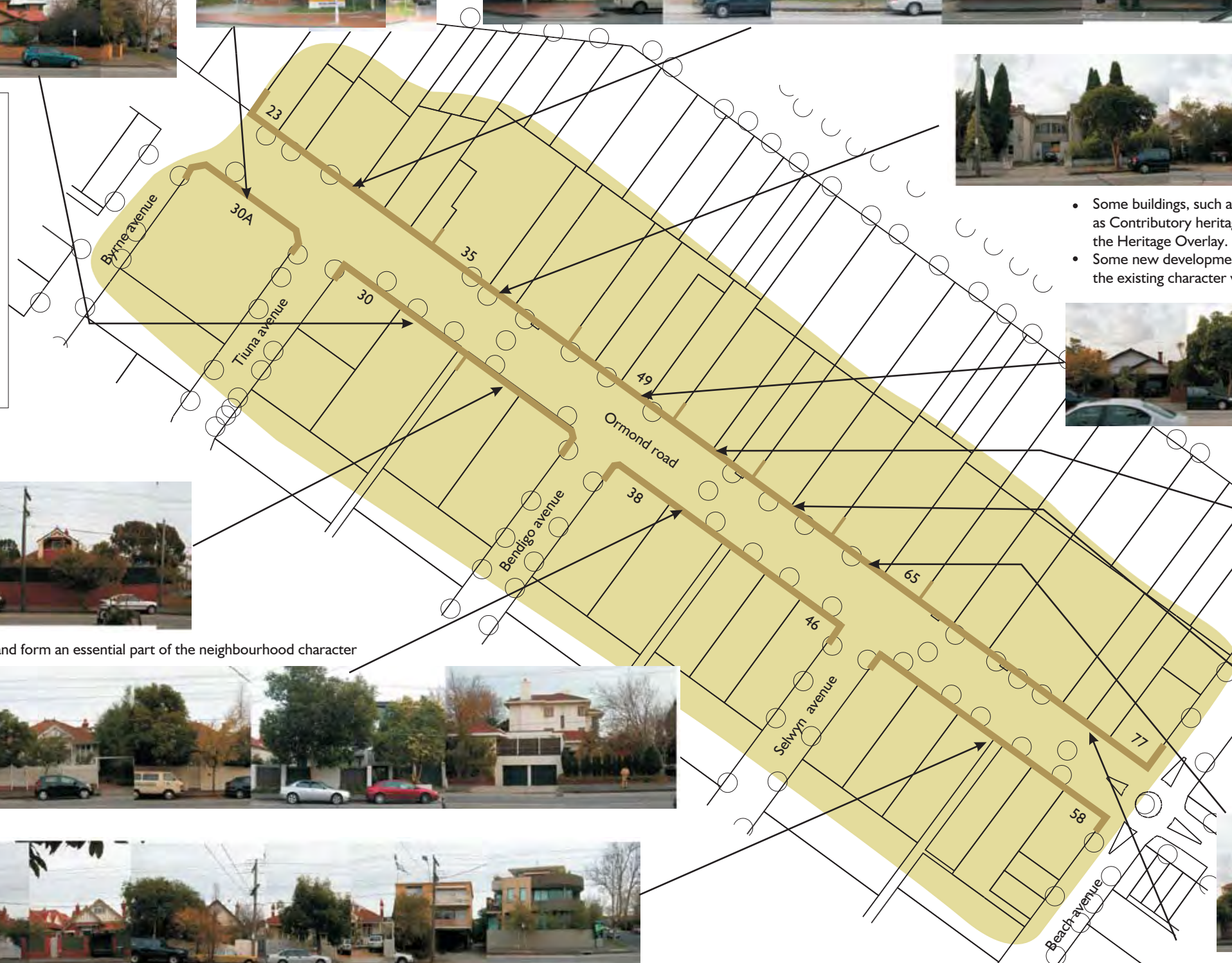


- Some buildings, such as those shown above left, are identified as Contributory heritage buildings while not being covered by the Heritage Overlay.
- Some new development, such as that above right, responds to the existing character with a similar building typology

## Residential Precinct 1

This precinct is characterised by:

- Residential I zoning
- Heritage Overlay in parts
- Detached dwellings and walk-up flats from the Inter-war period.
- Mixed building heights of single, double and three storeys
- Mixture of flat and pitched, terracotta roofs
- Small front and side setbacks; larger rear setbacks.
- Large deciduous trees and shrubbery along the street and low front fences in various materials



- These villas have significant heritage and form an essential part of the neighbourhood character



- These villas have significant heritage value and form an essential part of the neighbourhood character



- A series of walk-up apartments from the Inter-war period



- Many of the larger lots, such as those shown above right, have undergone redevelopment into more contemporary 3-storey buildings
- Detached dwellings from the Inter-war period, with pitched terracotta roofs, sit alongside, as shown above left
- Deciduous trees and shrubbery occurs intermittently along the street
- Low front fences are built in brick, metal or wood



PPH005 / 003

Scale: N.T.S



# RETAIL PRECINCT 2



- New buildings of contemporary character sit alongside Inter-war heritage buildings creating a mixed character
- Active street corners with outdoor cafes, dining places and verandahs



- Recent 5-storey development with a 4-storey front wall alongside older 3-storey buildings



- Active shopfronts
- Masonry external walls with punched windows
- Extended parapets of different shapes add character to the street
- Mix of styles from Inter-war to contemporary



- One of the stretches of consistent Inter-war heritage character with higher floor-to-floor heights and a high parapet that conceals the roof



- A recent 5-storey development out of scale with the rest of the precinct



**Retail Precinct 2 - Elwood Village**  
 Elwood Village is a traditional strip shopping centre. This precinct is characterised by:

- Business 1 zoning
- Heritage Overlay in parts, underscoring the area's distinctive character of Inter-war buildings
- A mixed character of single, double and three-storey buildings. A few new four-five storey buildings add to the mix
- Buildings generally abutting the street except a few free-standing apartment buildings on larger lots
- Active shopfronts and verandahs
- Buildings with masonry external walls, punched windows and parapets which shape the skyline
- Varied lot sizes, with those on the south side markedly narrower than those on north
- Outdoor cafes and dining places
- Angled and parallel street parking

PPH005 / 004  
 Scale 1:2500

# RESIDENTIAL PRECINCT 2



- This entire stretch of buildings is identified 'Significant' in the Heritage Overlay heritage and forms an essential part of the neighbourhood character
- Double storey development shown below left is atypical of an area consisting of largely single-storey detached dwellings

**Residential Precinct 2**  
 This precinct is characterised by:

- Residential 1 zoning
- Heritage Overlay in part
- A high proportion of the buildings deemed either Significant or Contributory heritage places within the Heritage Overlay
- Single-storey detached dwellings from the Inter-war period. Double and three-storey developments at street corners acting as 'gateways' to the precinct
- Pitched terracotta roofs
- Small front and side setbacks; larger rear setbacks.
- Large deciduous trees and shrubbery along the street
- Low front fences in various materials such as brick, metal or wood



- Substantial number of large deciduous trees line the street
- Low front fences in various materials such as brick, metal or wood



- 3-storey developments create a 'gateway' at the entry to the precinct from St Kilda Road



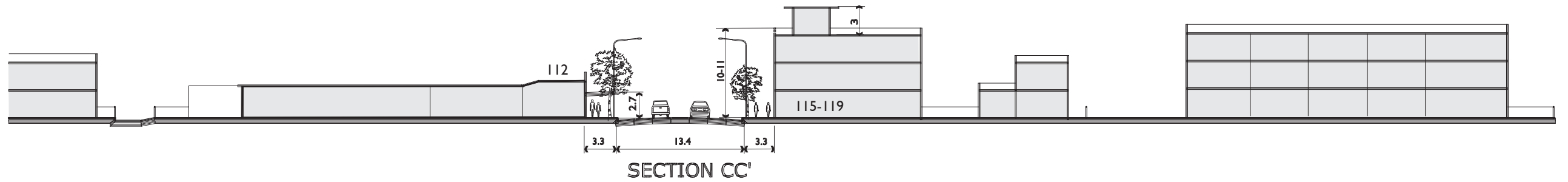
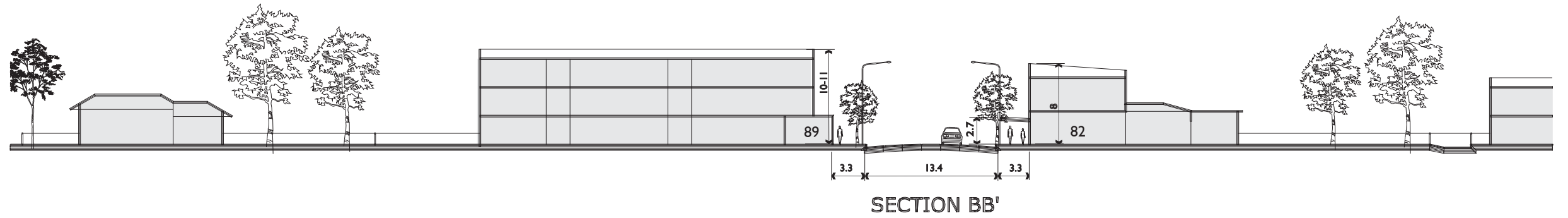
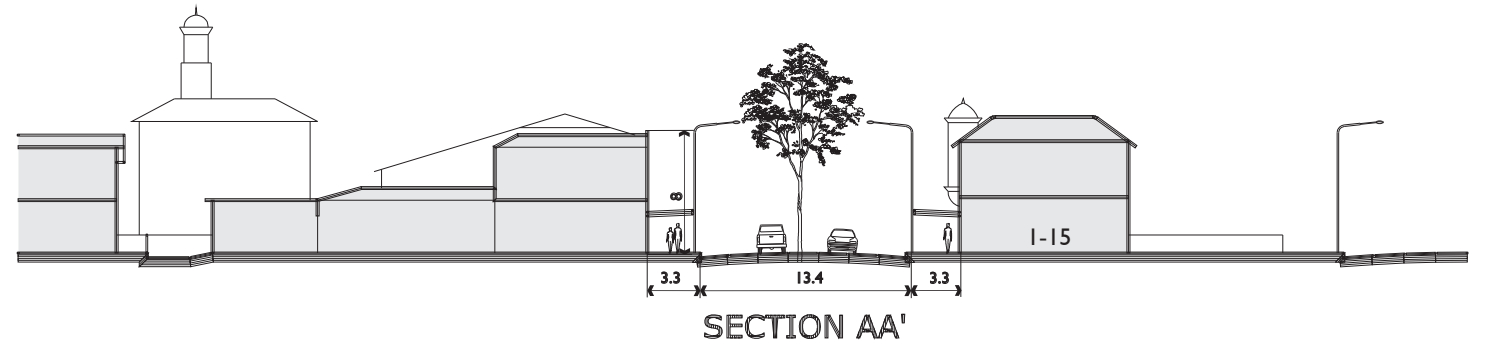
- Corner site cleared for a 4 storey development at the entry to the precinct from St Kilda Street

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 Scale: N.T.S

# TYPICAL STREET CROSS-SECTIONS



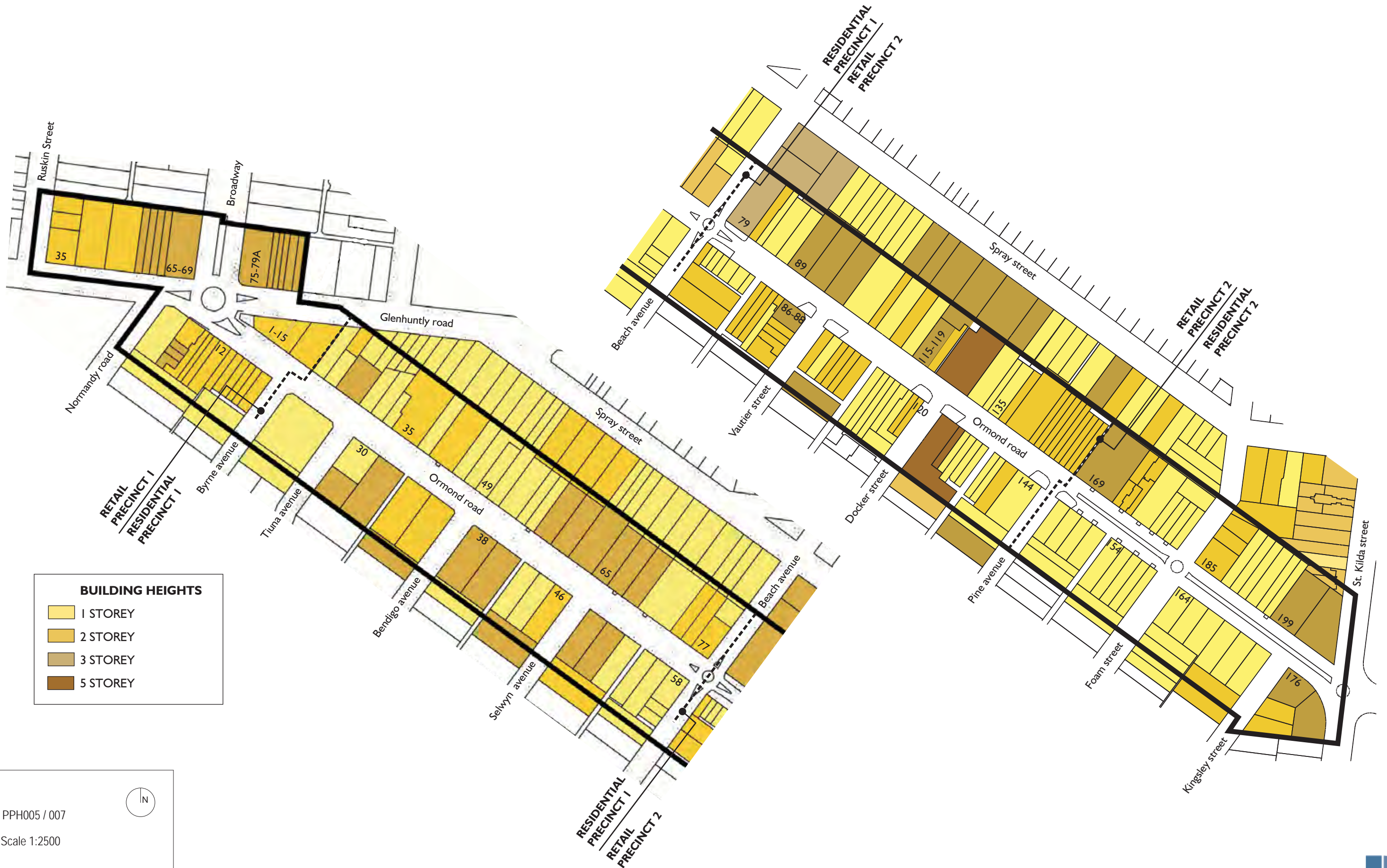
This drawing indicates the typical built form conditions currently found within the study area.



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Scale 1:500

August 08

# EXISTING BUILDING HEIGHTS



**BUILDING HEIGHTS**

- 1 STOREY
- 2 STOREY
- 3 STOREY
- 5 STOREY

PPH005 / 007

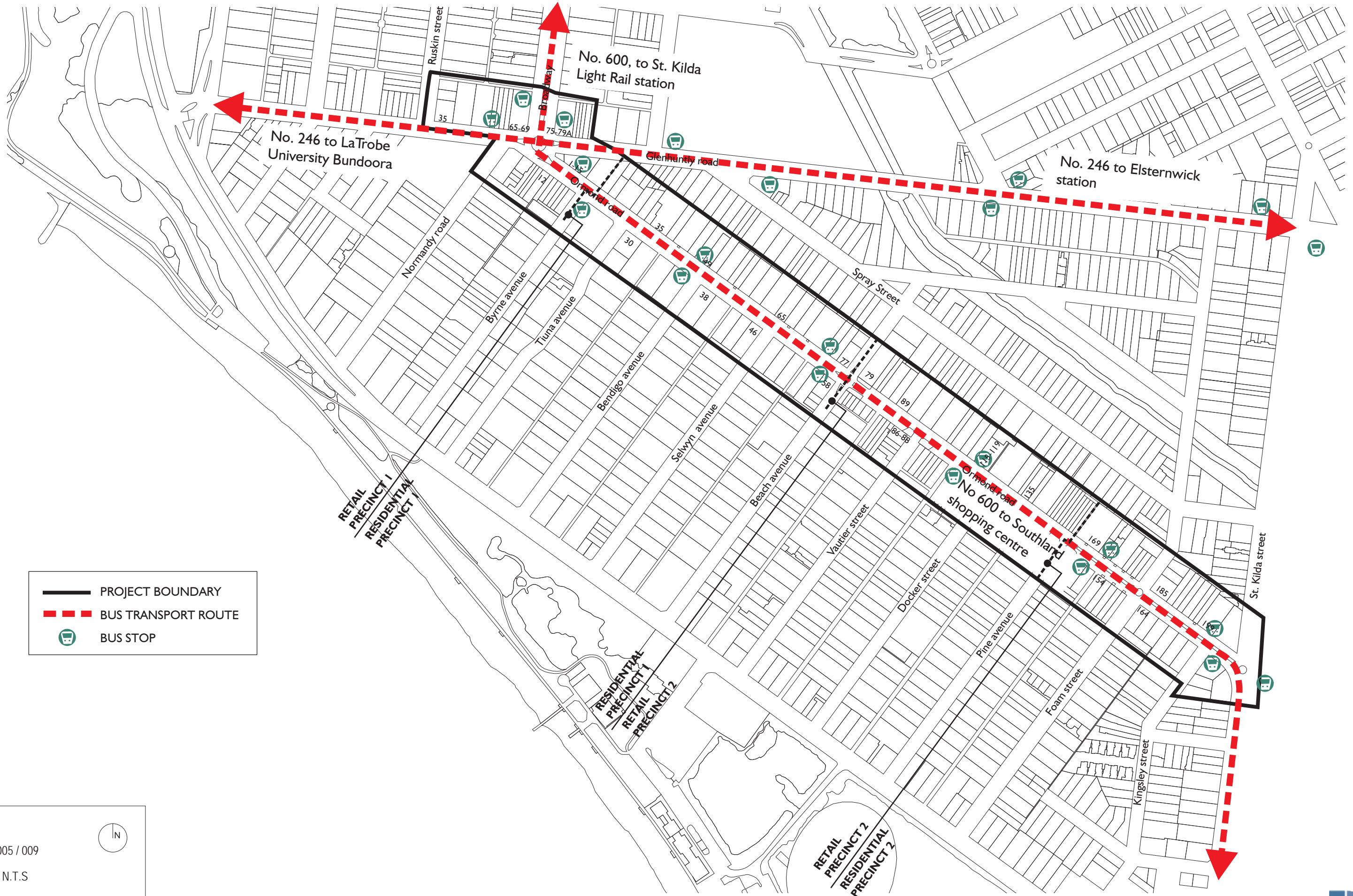
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August 08

## ORMOND ROAD URBAN DESIGN GUIDELINES



# PUBLIC TRANSPORT



PROJECT BOUNDARY  
 BUS TRANSPORT ROUTE  

 BUS STOP

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 Scale N.T.S.

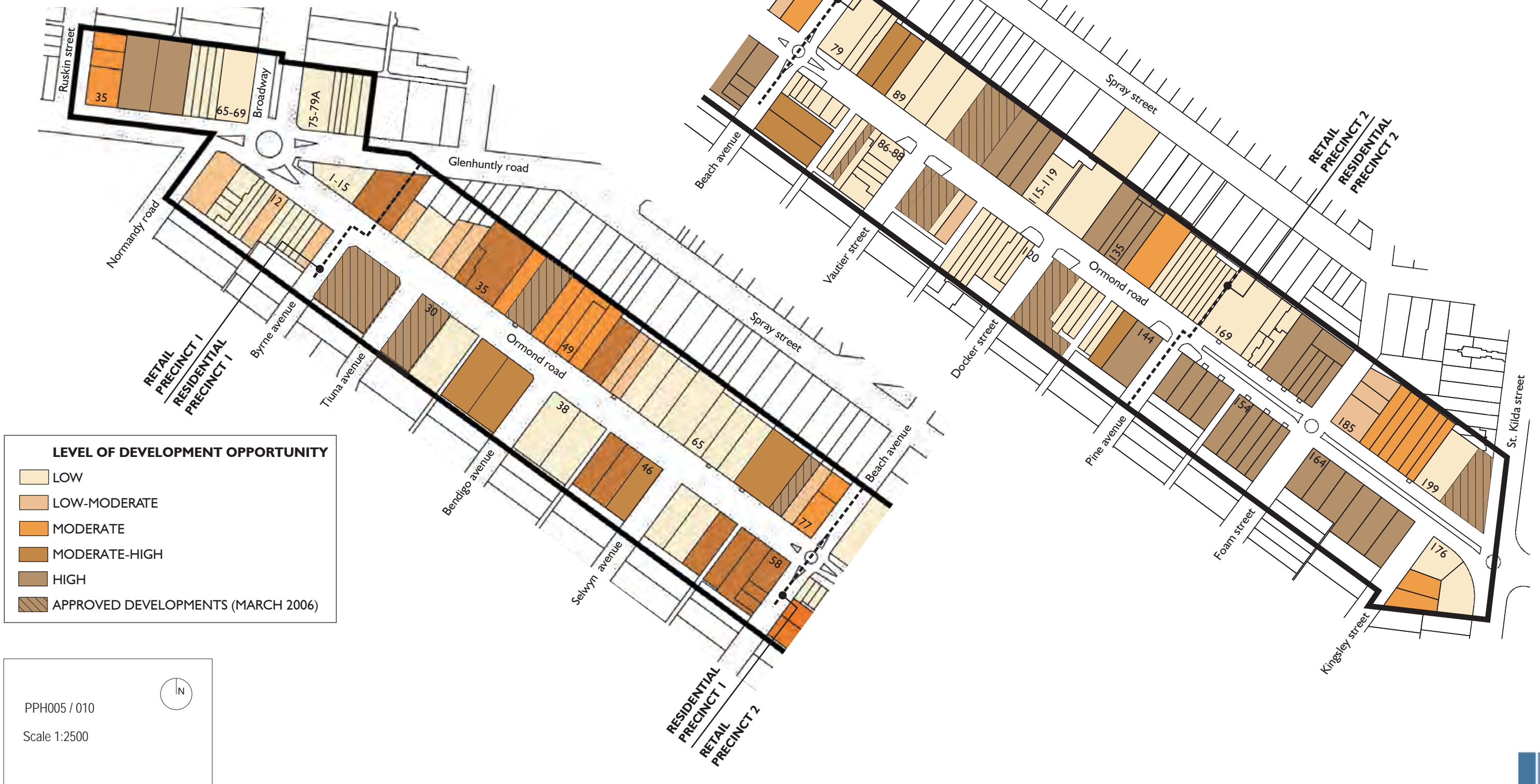
August 08

# LEVEL OF DEVELOPMENT OPPORTUNITY

This drawing indicates the likelihood of each property being redeveloped, based on an analysis of the factors that may constrain development, including:

- Age and height of existing development
- Strata titles
- Lot size
- Heritage values

The purpose of this analysis is to understand the likely pattern of redevelopment. It is not intended to limit the development potential of individual properties.






The following sheets provide recommendations regarding built form controls for future development in the project area. The plan below shows the properties to which the guidelines apply. Note that these Guidelines do not replace the need to comply with ResCode (where applicable) and heritage controls.



- Properties to which guidelines apply
- Properties to be rezoned from BI zone to RI zone

PPH005 / 011  
Scale N.T.S.



August 08

# ORMOND ROAD URBAN DESIGN GUIDELINES

## Statement of desired character

The existing rows of 2-storey, Inter-war shops in the retail precincts will form the basis for their future character. New development will be of a similar scale as viewed from the street. Façade treatments will continue the characteristic pattern of masonry materials, regular vertical articulation, verandahs, parapets, and window proportions and locations present in the Inter-war shopfronts. However, new development will be of a contemporary character and will not mimic their architectural style.

## Objectives

- To provide for additional office space and dwellings within the neighbourhood activity centre.
- To protect heritage values.
- To protect valued built form character.
- To protect the amenity of adjoining residential properties.
- To protect the amenity of the streets.

## Commentary

The proposed controls seek to allow some urban consolidation while maintaining the character of the centres. There are a number of 2-storey Inter-war shops within both retail precincts, which make key contributions (and possibly the most important contributions) to its valued character. These have relatively high floor-to-floor heights compared with modern development. This allows the potential for 3-storey contemporary development within a similar built form scale.

The proposed controls require new development to be between 7.5 and 10 metres high at the street boundary, to maintain the existing built form character. This may allow three floors, or it may only allow two floors if high floor-to-floor heights are required for functional or ESD reasons.\*

While there are a number of single-storey buildings in the Elwood Village centre at present, it is considered that their redevelopment with buildings more consistent with the height of the 2-storey Inter-war buildings would help establish a more coherent character without unreasonably reducing the amount of 'sky view' or sunlight reaching the footpaths.

If further floors above this 3-storey height are set back sufficiently so that, at most, only fascia and eaves are visible from directly across the street, it is considered that they will not adversely affect the local character or public realm amenity. It is considered, however, that additional floors that are highly visible from the street would have greater adverse impact on the local character and public realm amenity, and therefore are not justified given the lack of rail-based public transport provision. An exception to this is made for architectural features at street corners, which continue the pattern established by the heritage buildings at Elwood Junction.

No development is allowed above 10 metres high at the corner of Glenhuntly Road and Ruskin Street, as this would create an unreasonably abrupt increase in height from the single-storey dwellings to the west. Further, none of the built form principles outlined above apply to 129 and 131 Ruskin Street, as these properties shall be rezoned to Residential 1 zone.\*

The properties in the retail precincts abut residential properties at the rear. Applying ResCode for visual bulk and overshadowing will ensure that the amenity of those residential properties is considered. While development in the study area may exceed 10 metres in height, any part of development on Ormond Road above 10 metres is required to be invisible from the rear of the abutting dwellings that have back-on relationship with subject land. It is not considered necessary that this additional constraint be applied to lots abutting dwellings that have a 'side on' relationship with the subject area and it is not considered reasonable for their occupants to have the same expectation with respect to visual bulk to the side.\*

\* Refer to Foreword



Artists impression of possible future development in Ormond Road according to proposed guidelines

# RETAIL PRECINCTS - GENERAL GUIDELINES

Standards	Design suggestions
The front wall should have a height between 7.5 and 10 metres and be built to the front boundary for the full width of the lot.*	A height of 10 metres may allow three floor levels.*
Development may extend up to a total height of 13 metres, except at 35 Glenhuntly Road. Development above 10m should be set back from the front boundary sufficiently to ensure that, at most, only fascia and eaves are visible from standing eye-level on the opposite footpath. This setback also applies to the side street boundary on corner lots.	A front setback of approximately 7 metres to a further floor above the height of the front wall will avoid it being visible from across Ormond Road. If this setback is to be used as a terrace, setting the balustrade back approximately 2 metres from the front wall will avoid it being visible from across the street.
Architectural features up to 13 metres high are encouraged on street corners, except the corner of Glenhuntly Road and Ruskin Street, provided that they are well integrated with the design of the building.	Introduce a roof feature (such as a dome) to express street corners. Relate the roof feature to the design of the building corner below, eg. through changes in materials and/or colours, and/or vertical articulation.

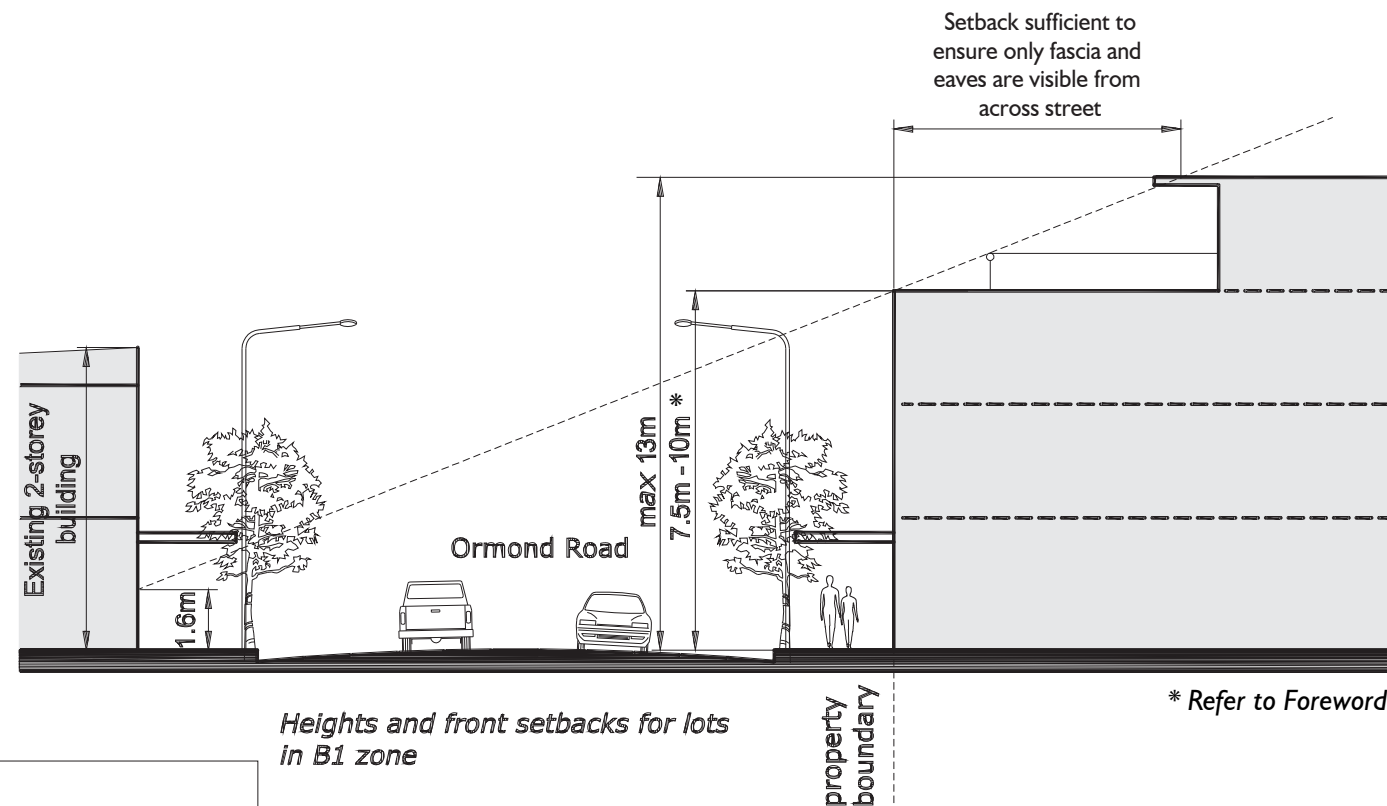
\* Refer to Foreword

Standards	Design suggestions
The ground floor must be at footpath level.	
The front wall should have a verandah for the full width and consistent with the height of adjoining verandahs, a parapet, vertical articulation and window proportions and locations that follow the characteristic pattern of the existing Inter-war shopfronts in the centre.	Use the characteristic rhythm of articulation to inform the internal layout. If three levels are to be accommodated within the height of the front wall, use the characteristic window forms as clear openings to balconies behind, to avoid a misalignment of the windows and floors.
The front wall should be finished in red face brickwork and/or light coloured render.	
The front wall should be designed to facilitate eye contact between occupants and people in the street.	Incorporate at least one pedestrian entrance and a substantial area of clear-glazing in the ground floor frontage, and incorporate windows in upper level frontages. Locate pedestrian entrances to upper level uses in the ground floor frontage if possible.
All vehicular access is to be provided from the side or rear, wherever possible. Additional crossovers or garage doors facing the street are not permitted except where there is no alternative.	

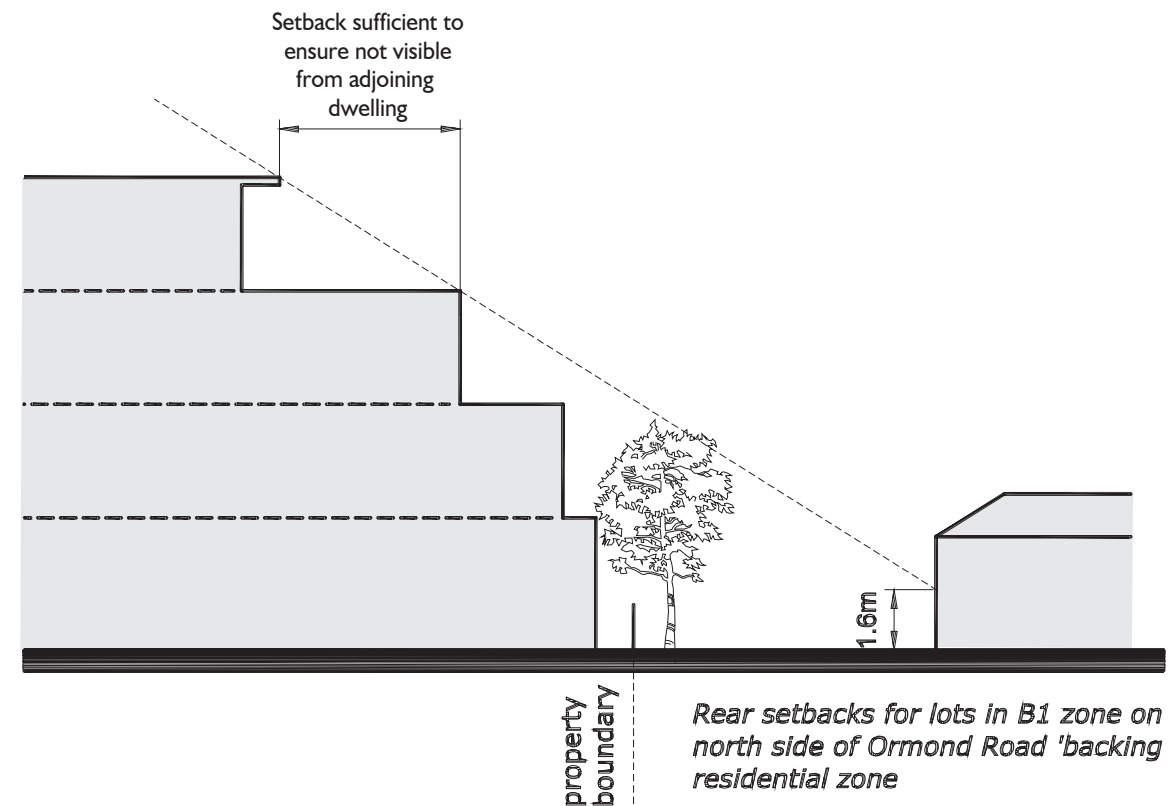
Standards	Design suggestions
Exposed side walls should be well articulated.	Incorporate changes of material and/or colour in exposed side walls. Incorporate doors, windows, balconies, porches, bay windows, overhanging eaves, recesses and/or projections in side street facades.
Rear setbacks should comply with Clause 54.04-1/55.04-1 of the Port Phillip Planning Scheme (side and rear setbacks objective).	Setback upper levels to comply with Clause 54.04-1/55.04-1. Where the rear of a site abuts the rear boundary of adjoining residential development or faces it across a laneway (on the north side of Ormond Road), set back any development over 10 metres high so that it is not visible from standing eye-level at the rear of the adjoining development.*
Development should comply with Clause 54.04-5/55.04-5 of the Port Phillip Planning Scheme (overshadowing open space provision).	

Note: Lots 129 and 131 Ruskin Street should comply with ResCode provisions after completion of their rezoning process to Residential 1 zone.

\* Refer to Foreword



Heights and front setbacks for lots in B1 zone

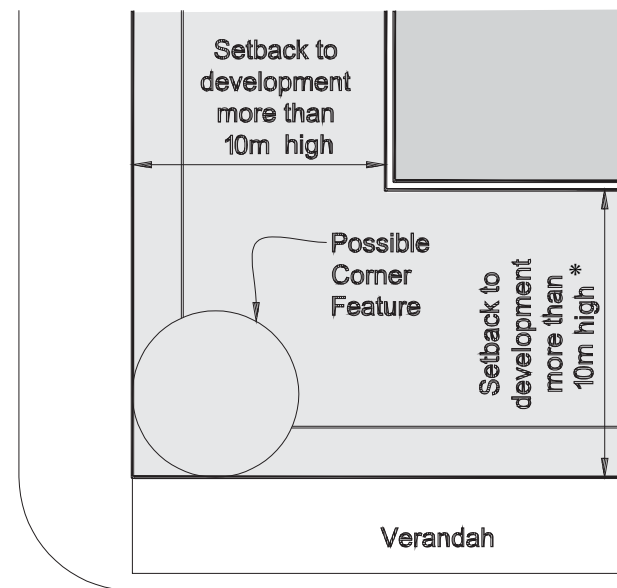
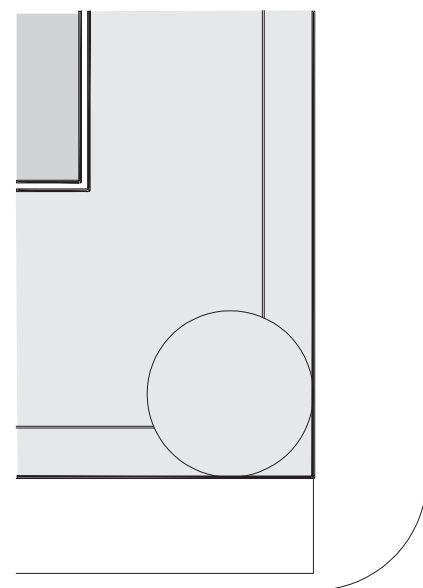
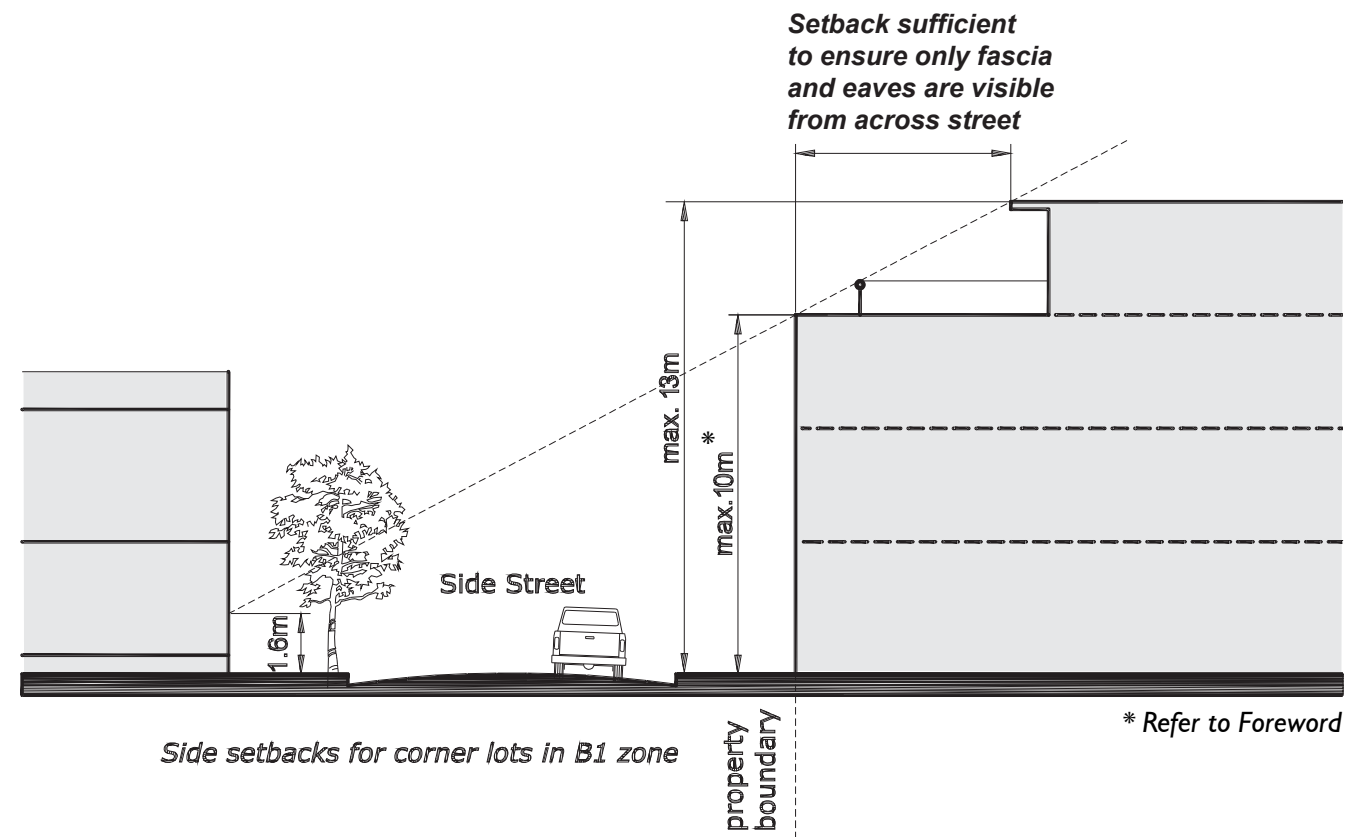
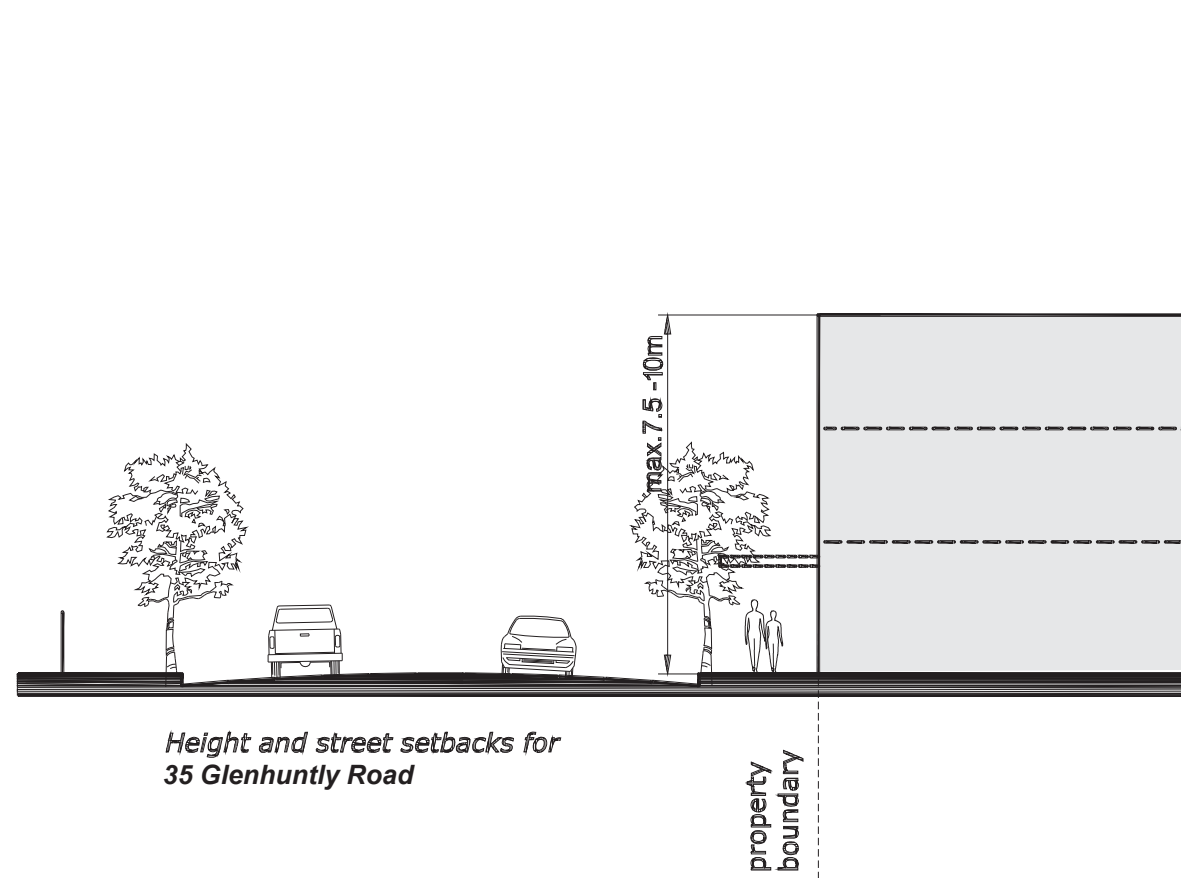


Rear setbacks for lots in B1 zone on north side of Ormond Road backing residential zone

PPH005 / 012

Scale 1:200





*Plan showing front and side setbacks for corner lots more than 10m high in B1 zone*

\* Refer to Foreword

PPH005 / 013

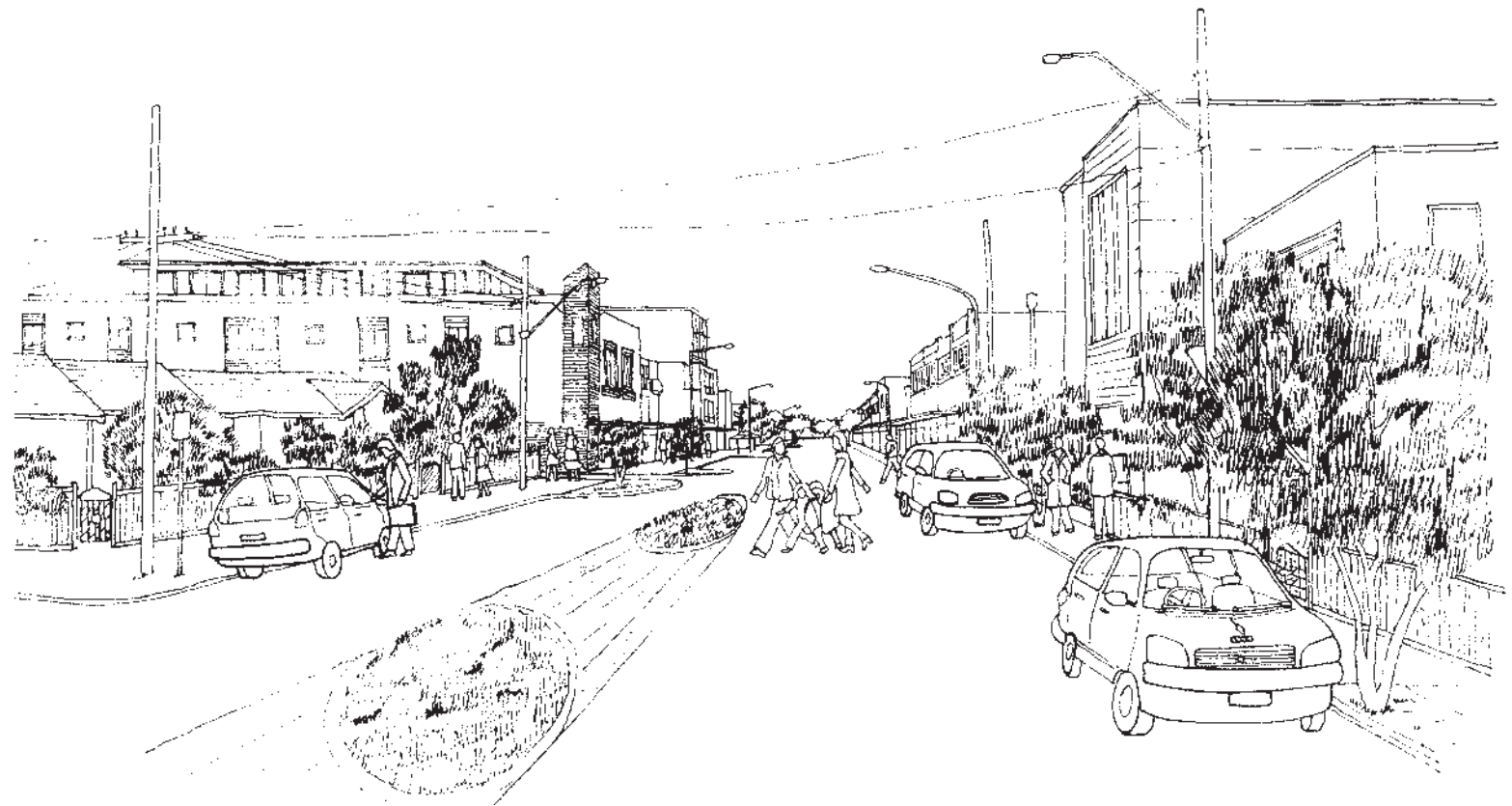
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August 08





*Artists impression of possible future development at Pine Avenue / Ormond Road intersection according to proposed guidelines. View towards north along Pine Avenue*



*Artists impression of possible future development at Pine Avenue / Ormond Road intersection according to proposed guidelines. View towards west along Ormond Road*

PPH005 / 014  
Scale N.T.S

August 08



**Statement of desired character**

Approximately, a third of the buildings in the residential precinct are 3 storeys in height. New development will be of a similar scale as viewed from across the street. However, it will be of a contemporary character and highly articulated, rather than the 'flat-fronted' buildings common in the area now. New development will be set back from the front boundary to continue the 'leafy' character of this area by allowing space for substantial planting in the street and front garden.

**Objectives**

- To provide for additional dwellings close to the neighbourhood activity centre.
- To protect heritage values.
- To create a more consistent built form character.
- To protect the amenity of adjoining residential properties.
- To protect the amenity of the streets.

**Commentary**

The proposed controls seek to allow some urban consolidation without significantly changing the character of the centre. There are a number of 3-storey apartment buildings within these areas, which dominate their character and allow a reasonable amount of 'sky view' and sunlight reaching the footpaths.

If further floors above this height are set back sufficiently so that they are not visible from directly across the street, it is considered that they will not adversely affect the local character or public realm amenity. It is considered, however, that additional floors that are highly visible from the street would have greater adverse impact on the local character and public realm amenity, and therefore are not justified given the lack of rail-based public transport provision.

The proposed controls set a maximum height for new development to 13 metres which allows for four storeys. This height is generally sufficient to achieve good ESD outcomes in smaller footprint buildings typical in suburban situations.

All buildings on Ormond Road are required to be setback a minimum of 6 metres from the front boundary or as required by *ResCode*, whichever is greater. This will ensure protection of amenity of the streets and creation of more consistent built form character in the residential precincts.

The properties in the residential areas abut more residential properties at the rear. Applying *ResCode* objectives for visual bulk and overshadowing will ensure that the amenity of those residential properties is considered. Any development on land on Ormond Road above 10 metres is required to be invisible from the rear building line of any adjoining residential property.

It is not considered necessary that this additional constraint be applied to lots that have a 'side on' relationship with the subject site and it is not considered reasonable for their occupants to have the same expectation with respect to visual bulk to the side.\*

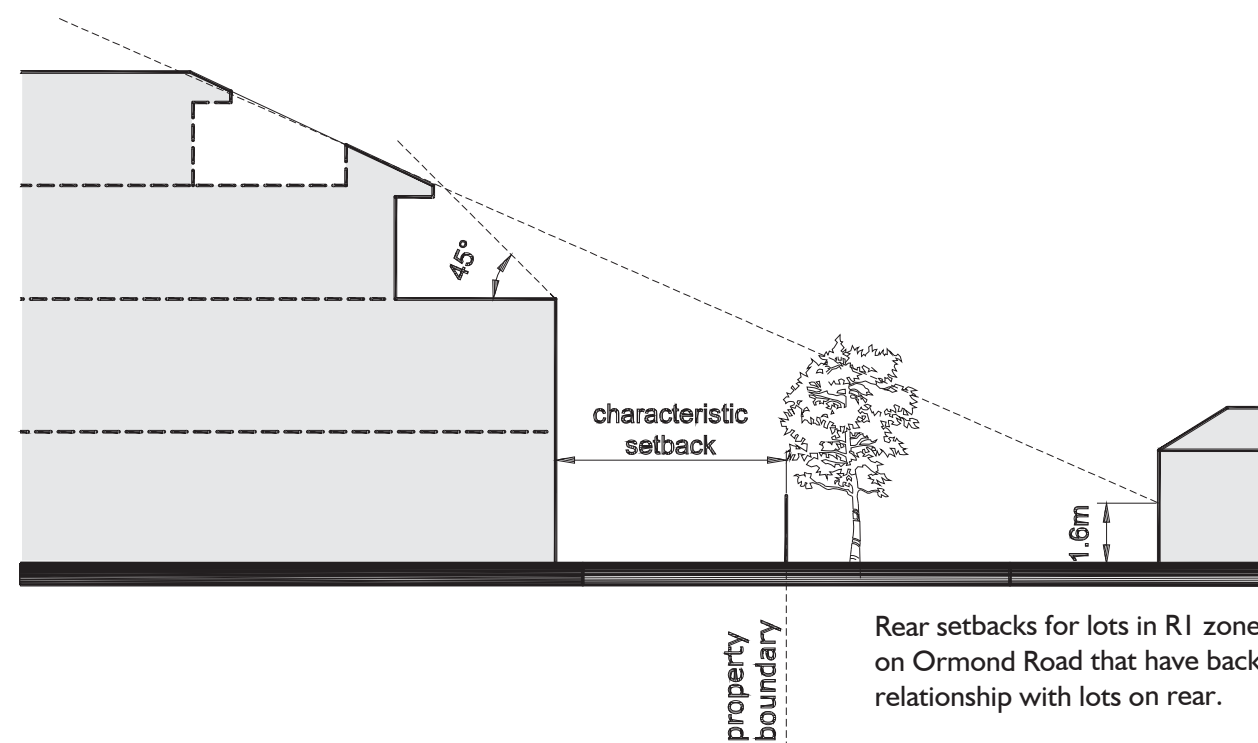
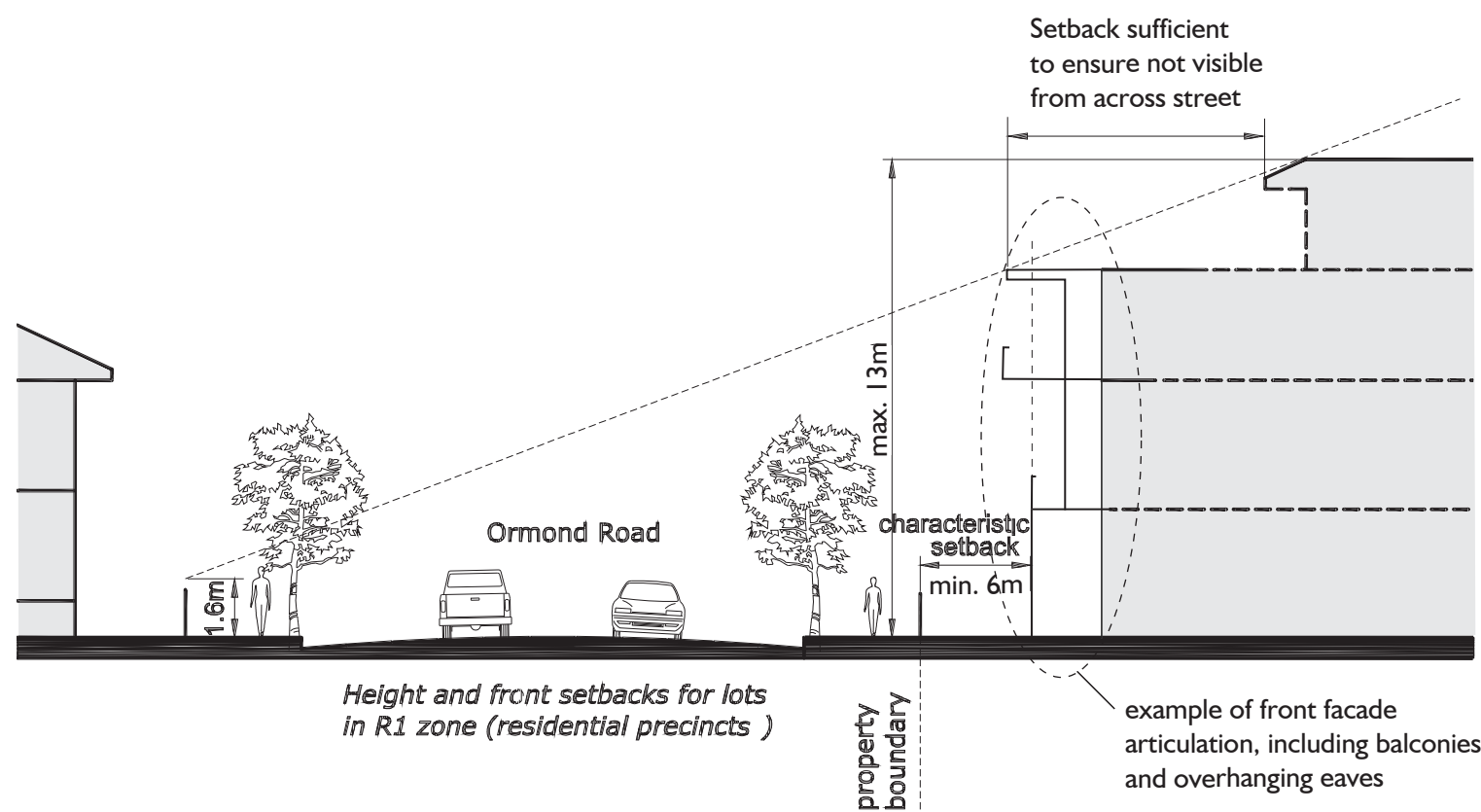
\* Refer to Foreword

# RESIDENTIAL PRECINCTS - GUIDELINES

Standards	Design suggestions
Development should be set back from the front boundary of the lot a minimum distance of 6 metres in accordance with Clause 54.03-1/55.03-1 of the Port Phillip Planning Scheme (front setback provision), whichever is greater.	
Development should have side setbacks that conform with the character of the area.	
Development may be built up to 13 metres high, provided that any part more than 10 metres high is not visible from adjoining streets or the rear building line of adjoining residential properties.*	A front setback of approximately 7 metres to a further floor above the height of the front wall will avoid it being visible from across Ormond Road. If this setback is to be used as a terrace, setting the balustrade back approximately 2 metres from the front wall will avoid it being visible from the street.

\* Refer to Foreword

Standards	Design suggestions
The front wall should contain windows and be articulated to create an attractive facade. This standard also applies to walls facing side streets.	<i>Incorporate front doors, windows, balconies, porches, bay windows, overhanging eaves, recesses, projections, changes of material and/or colour in the front facade.</i>
Development should have rear setbacks that maintain the characteristic pattern of backyards in the area.	<i>Base the rear setback dimension on the typical rear setbacks within 50 metres of the site in either direction.</i>
Development should comply with Clause 54.04-5 / 55.04-5 of the Port Phillip Planning Scheme (overshadowing open space provision).	



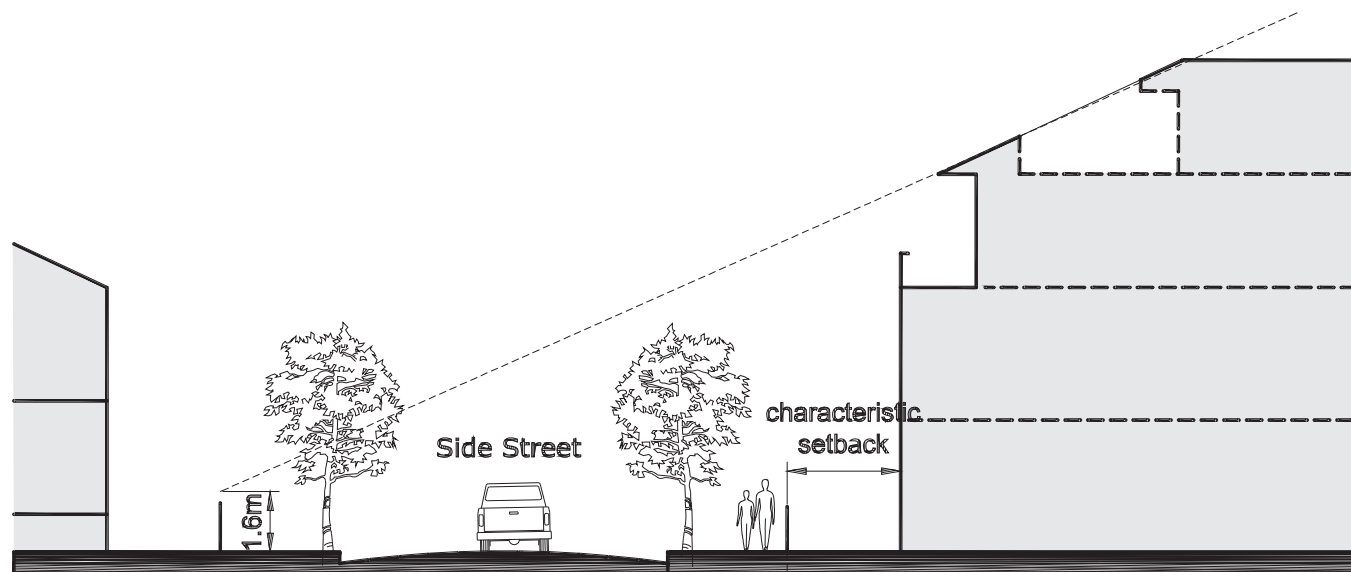
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August 08

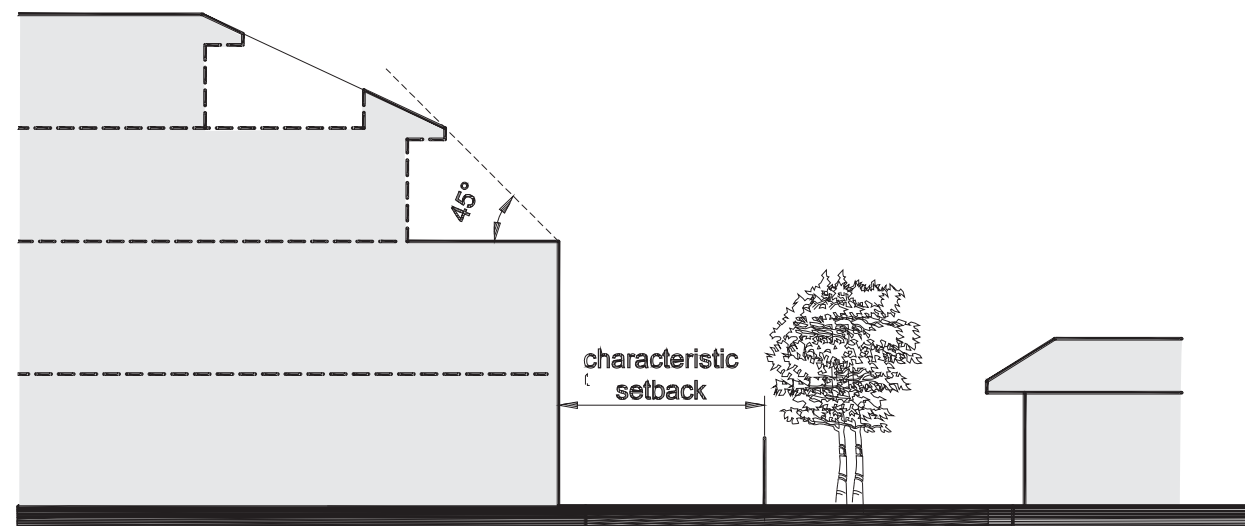


# RESIDENTIAL PRECINCTS - GUIDELINES (cont.)



Side setbacks for corner lots in R1 zone (residential precincts)

property boundary



Rear setback for lots in R1 zone on Ormond Road with side-on relationship with lots on rear

property boundary



Artists impression of possible future development at Bendigo Avenue/ Ormond Road intersection, according to proposed guidelines. View towards north along Bendigo Avenue

PPH005 / 016

Scale 1:200

August 08

